



**SAN MATEO**

# **COMPLETE STREETS NORTH CENTRAL PLAN**

**Program will start at 7:15 PM**



**FEHR PEERS**

**winter**



# AGENDA

- 1. What is the Complete Streets North Central Plan?**
- 2. Plan Approach**
- 3. Engagement Highlights**
- 4. Location Specific Comments**
- 5. Potential Improvements**
- 6. Next Steps**



# Group Guidelines

**Share the space**

**One speaker at a time**

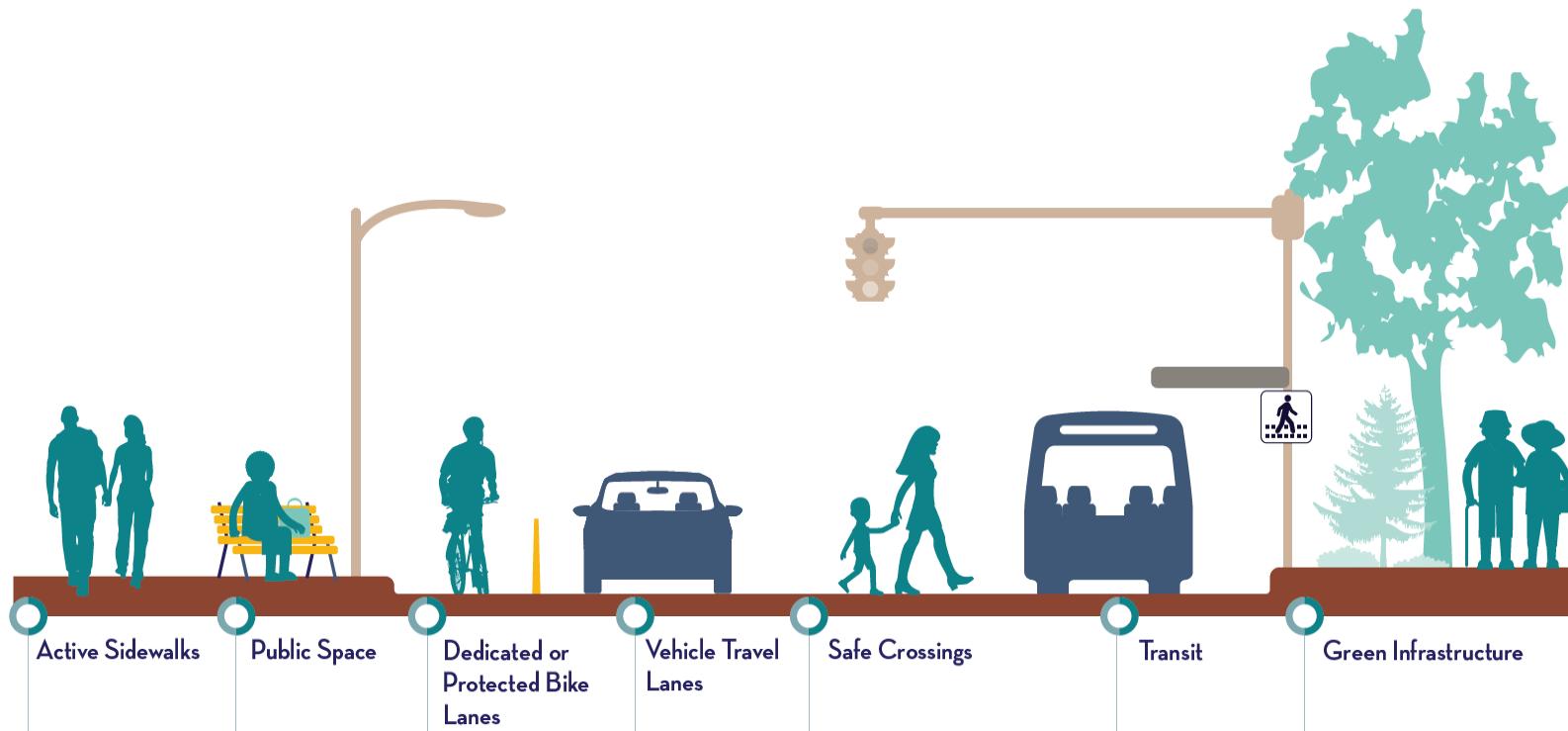
**Be mindful of time**

**Respect one another**



# What is a Complete Street?

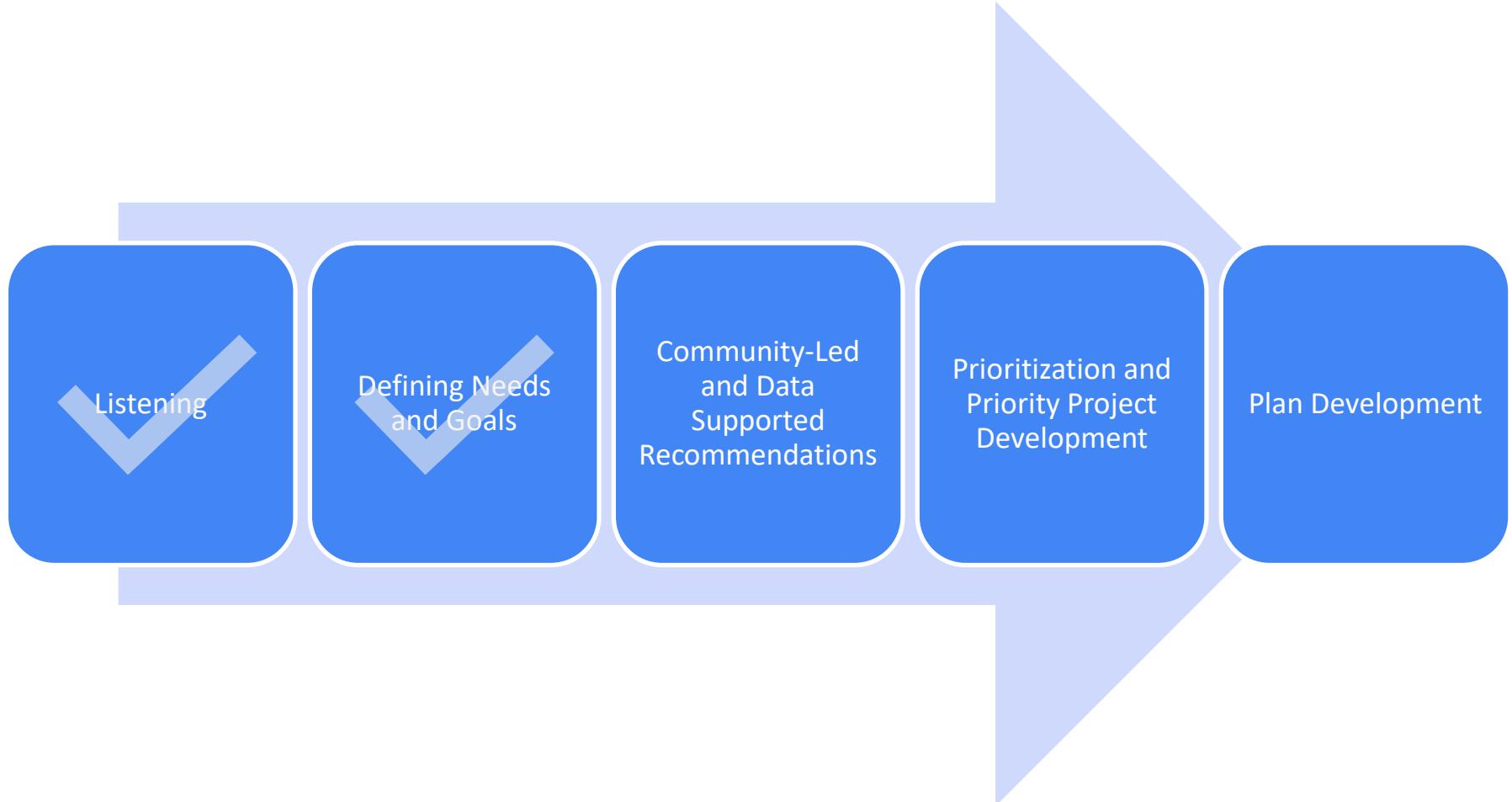
Streets that are safe, comfortable, and accessible **for all users** and **multiple forms of travel**



The Complete Streets North Central Plan focuses on developing a **safe, reliable, accessible, and equitable** multimodal transportation system by pursuing a collaborative community engagement process that encourages deep listening of community members and residents in North Central.



# Plan Approach

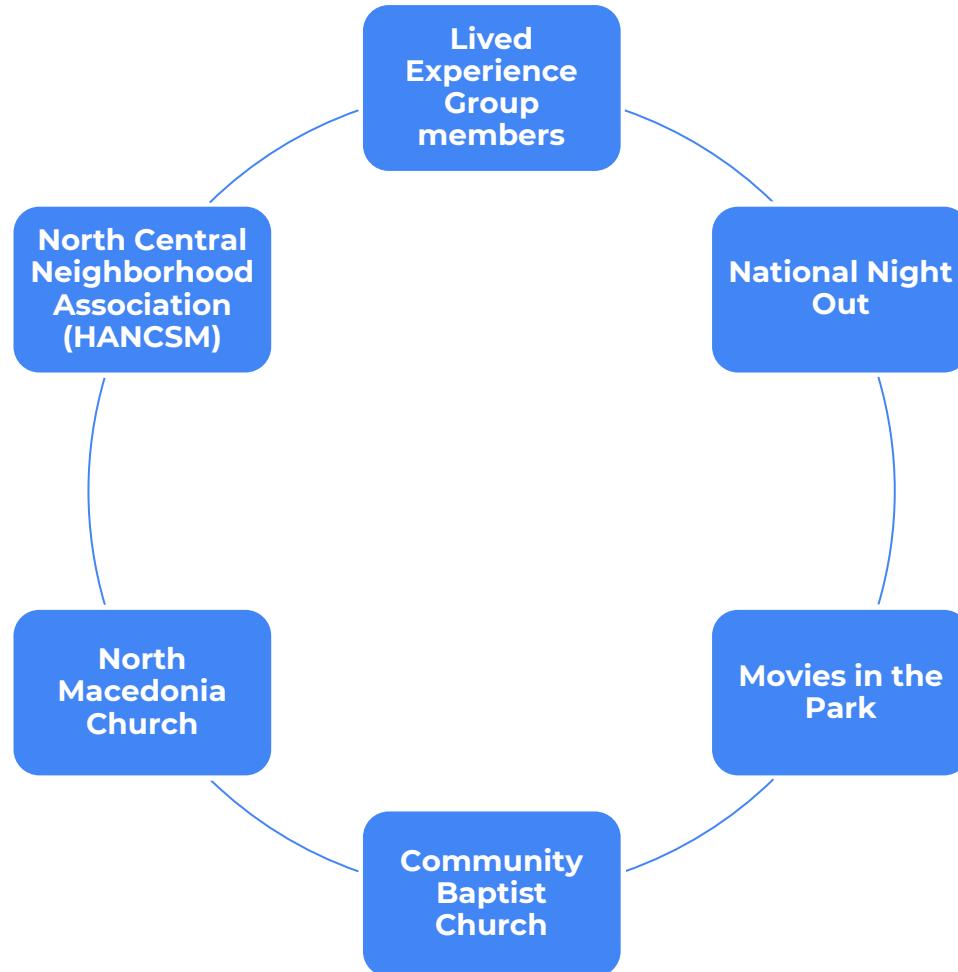


# Phase 1 Engagement Recap

- **Timeline: August 2023 – December 2023**
- **Overview**
  - **Over 200+** North Central residents engaged in person (in Spanish and English).
  - **3** focus groups, **4** pop up tabling events, **2** Lived Experience Group meetings
  - **Spanish interpretation** provided at most activities.



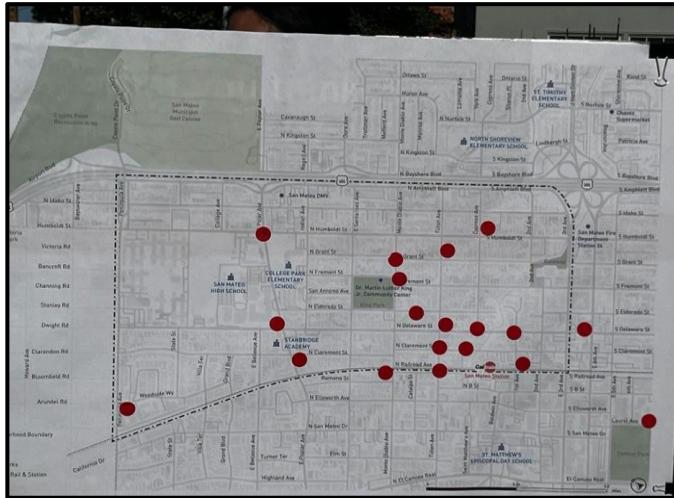
# Stakeholder Partnerships





# Lived Experience Group

2 Meetings and public engagement support



LEG meetings and community engagement events coordinated in support with LEG members



# Phase 1 Engagement

## August - December 2023



**National Night Out Pop Up  
(44 Participants)**

August 1, 2023



**Movies in the Park Pop Up  
(15 Participants)**

September 7, 2023



**Food Distribution Pop-Up at Macedonia Church of God-Christ  
(40 Participants)**

November 7, 2023



**HANCSM Meeting Pop In Focus Group  
(~40 Participants)**

November 9, 2023



**North Central Residence Focus Groups (2)  
(~20 Participants)**

November 30, 2023



**Community Baptist Church Pop-Up  
(~50 Participants)**

December 11, 2023



# Location-Specific Comments

City of San Mateo Complete Streets North Central

## Defining Community Needs and Goals

## Mapping of Community Needs and Goals Collected Through Community Engagement

DRAFT

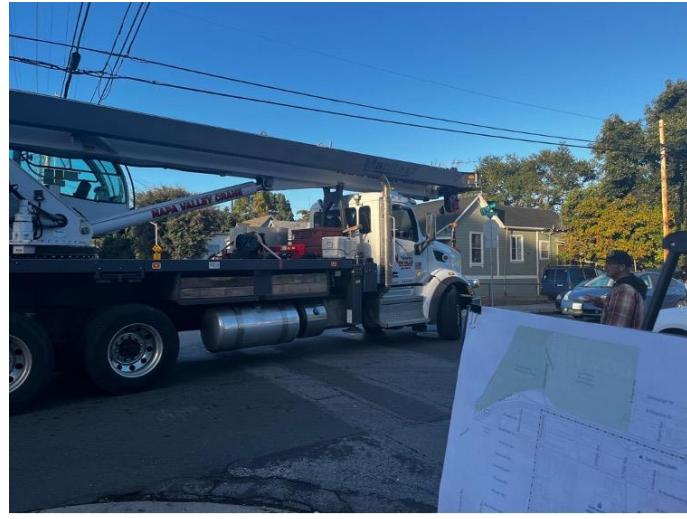




# Engagement Highlights

General themes from community feedback

- **Traffic Safety Concerns**
- **Visibility and Turning Safety**
- **Safety Infrastructure Improvements**
- **Limited Right of Way**
- **Neighborhood aesthetics and identity**
- **Public Transit Improvements**
- **Enforcement of Regulations**
- **Illegal Activities and Enforcement**
- **Pride in the Neighborhood and Martin Luther King Jr. Center/Park**





Marked bike lane

Buffered bike lane



#### Class II: Bike Lane

Provides a striped zone for one-way bike travel in a roadway



#### Class III: Bike Route

Provides for shared use with motor



Parking-separated bikeway with  
soft curbs

Parking-separated bikeway with  
raised concrete buffers/curbs



#### Class IV: Separated Bike Lane

Provides a separated right-of-way &  
inclusion use of driveway adjustments



Next: Context-Sensitive Potential  
Improvements and Trade-Offs

# Speed Lump/Cushion



## ADVANTAGES

- Effective in reducing speeds
- Maintains rapid emergency response times
- Relatively easy for bicyclist to cross

## DISADVANTAGES

- Maintenance can be challenging
- Vehicles with wide wheelbase can pass through the lump using the wheel cutouts
- Increased noise from vehicles accelerating

# Raised Crosswalk



## ADVANTAGES

- Effective in reducing speeds, though not to the extent of speed humps
- Maintenance easier than speed humps
- Improve safety for both vehicles and pedestrians

## DISADVANTAGES

- Increased noise
- Impact to drainage (stormwater) needs to be considered

# Rectangular Rapid Flashing Beacons (RRFB)



## ADVANTAGES

- Effective at initiating yields to pedestrians
- Often solar powered

## DISADVANTAGES

- Must be installed in addition to other traffic signage (school zone, pedestrian crossing sign)
- Restrictions on where they can be installed relative to other signage or traffic control devices (signals, stop signs)
- May require power source or stop working if solar power is insufficient

# Relocation of Bike Lanes

**Legend**

**Bike Facility**

- Completed Bicycle Boulevard
- Completed Bike Lane
- Completed Buffered Bike Lane
- Completed Shared Use Path
- Proposed Bicycle Boulevard
- Proposed Bike Route
- Proposed Bike Lane
- Proposed Buffered Bike Lane
- Proposed Separated Bike Lane
- Proposed Shared Use Path

**Class II: Bike Lane**  
Provides a striped lane for one-way bike travel on a roadway

**Class III: Bike Route**  
Provides for shared use with motor vehicle traffic

**Class IV: Separated Bikeway**  
Provides a separated right-of-way for the exclusive use of bicyclists adjacent to a roadway

**Standard bike lane**

**Buffered bike lane**

**Parking-separated bikeway with soft-hit posts**

**Parking-separated bikeway with raised concrete buffer/tree planting**

## ADVANTAGES

- Manages traffic
- Reduces conflicts

## DISADVANTAGES

- Must consider impacts on other streets; challenges/issues displaced
- Can increase volume and speed of traffic on vacated street requiring additional traffic calming measures due to less friction

# One-Way/Two-Way Street Conversions



## ADVANTAGES

- Manages traffic
- Reduces conflicts

## DISADVANTAGES

- Must consider impacts on other streets
- Can increase volume and speed of traffic requiring additional traffic calming measures due to less friction
- Increase travel distances; operates best in pairs



# **DISCUSSION and QUESTIONS**



## **NEXT STEPS**

### **Prioritization and**

### **Priority Project**

### **Development**

1. Focus Groups/Pop Ups
2. Door Knocking
3. Community Workshop #3



# THANK YOU!

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